



TRANSPORTATION CABINET

Frankfort, Kentucky 40622
www.transportation.ky.gov/

Steven L. Beshear
Governor

Michael W. Hancock, P.E.
Secretary

November 10, 2011

CALL NO. 101
CONTRACT ID NO. 111044
ADDENDUM # 1

Subject: Pike County, APD 0806 (039)
Letting November 18, 2011

- (1) Revised - Plan Sheets - R2C, R2D, R44, S1, & S3
- (2) Revised - Special Notes - Pages 22-36(d) of 132
- (3) Added - Erratum - Page 112(a) of 132
- (4) Revised - Bid Items - Pages 129-132 of 132

Proposal revisions are available at <http://transportation.ky.gov/contract/>.
Plan revisions are available at <http://www.lynnimaging.com/kytransportation/>.

If you have any questions, please contact us at 502-564-3500.

Sincerely,

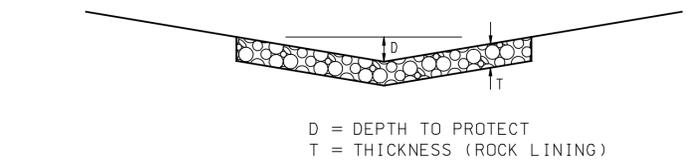
A handwritten signature in blue ink that reads "Ryan Griffith".

Ryan Griffith
Director
Division of Construction Procurement

RG:ks
Enclosures

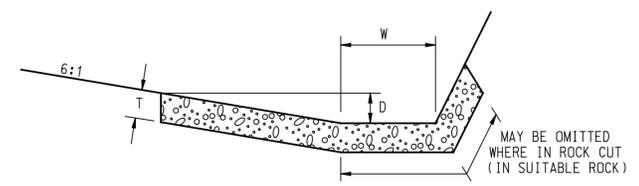


An Equal Opportunity Employer M/F/D



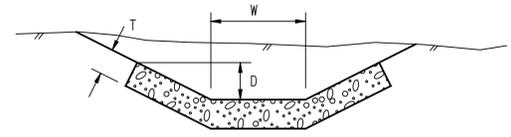
MEDIAN DITCHES

D = DEPTH TO PROTECT
T = THICKNESS (ROCK LINING)



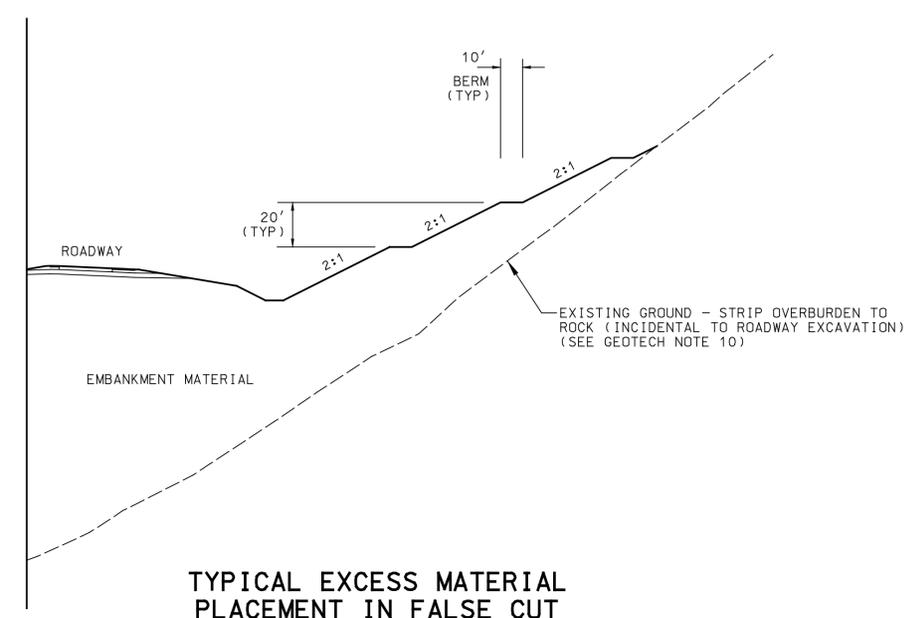
TYPICAL ROADWAY DITCH IN CUT SECTIONS

D = DEPTH TO PROTECT
T = THICKNESS (ROCK LINING)
W = DITCH WIDTH PLUS BENCH WIDTH (IF ANY)

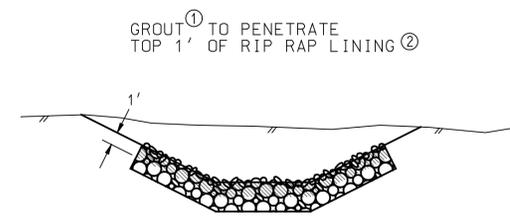


TYPICAL SURFACE DITCH SECTION

D = DEPTH TO PROTECT
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W = WIDTH

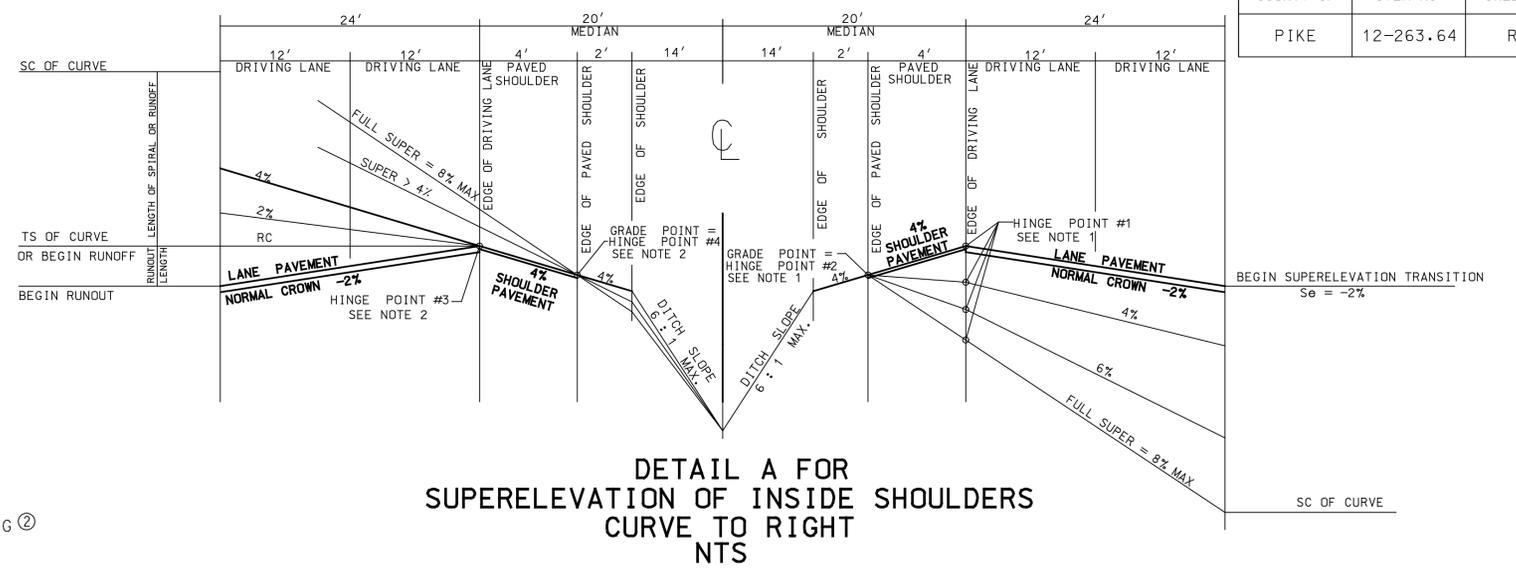


TYPICAL EXCESS MATERIAL PLACEMENT IN FALSE CUT NTS



① HIGH SLUMP 3000 PSI GROUT
② PLACEMENT OF GROUT SHOULD LEAVE TOP 1' LAYER OF STONES 1/3 TO 1/4 EXPOSED, AND RESULT IN A SURFACE MATRIX OF ROUGH STONE AND FILLED VOIDS

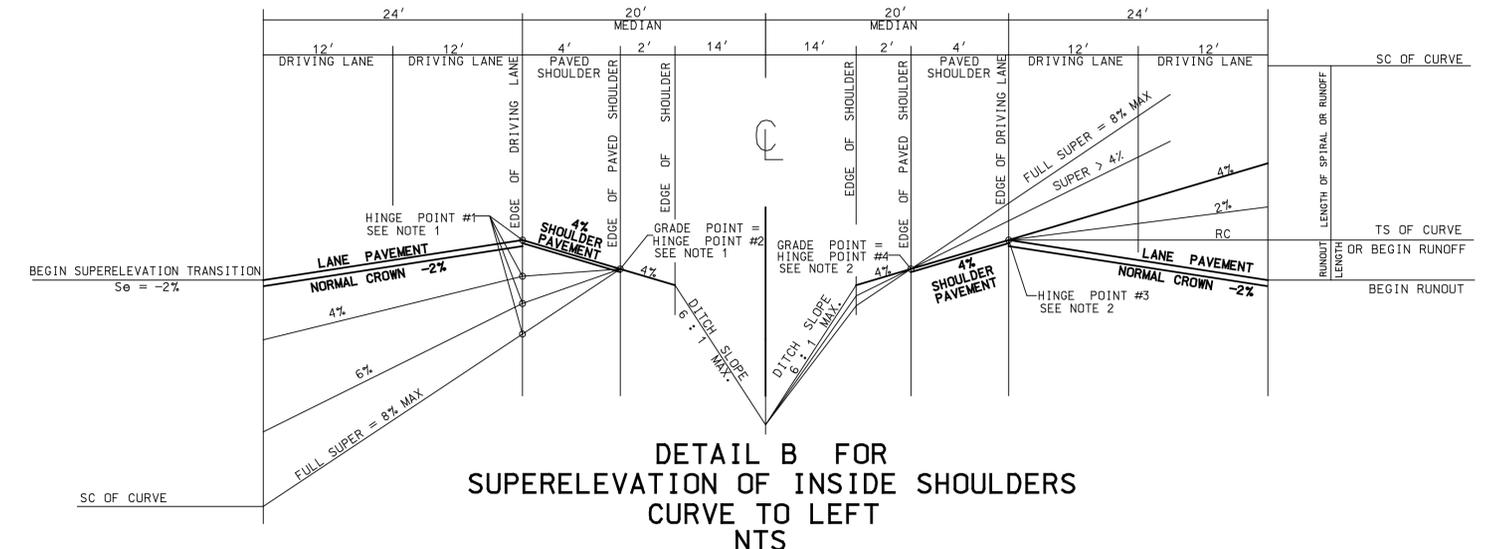
GROUTED RIP RAP DITCH SECTION



DETAIL A FOR SUPERELEVATION OF INSIDE SHOULDERS CURVE TO RIGHT NTS

NOTE #1: THE SHOULDER TRANSITIONS FROM POSITIVE 4% TO FULL SUPER ROTATING AROUND HINGE POINT #2 IN THE SAME DISTANCE THAT THE PAVEMENT TRANSITIONS FROM NEGATIVE 2% TO FULL SUPER ROTATING AROUND HINGE POINT #1

NOTE #2: FOR SUPERELEVATION FROM NORMAL CROWN THROUGH POSITIVE 4% THE DRIVING LANES ROTATE AROUND HINGE POINT #3 AND THE SHOULDER REMAINS AT NEGATIVE 4%. FOR SUPERELEVATION FROM POSITIVE 4% THROUGH POSITIVE 8% THE DRIVING LANES AND THE SHOULDER ROTATE AROUND HINGE POINT #4 WITH NO BREAK IN SLOPE AT THE EDGE OF PAVEMENT.



DETAIL B FOR SUPERELEVATION OF INSIDE SHOULDERS CURVE TO LEFT NTS

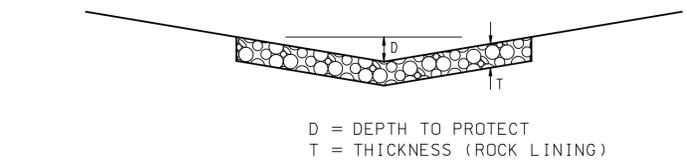
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PREPARED BY _____ DATE _____
CHECKED BY _____ DATE _____
APPROVED BY _____ DATE _____

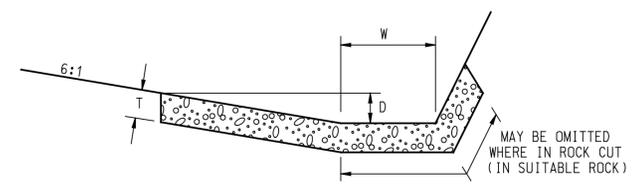
Cell Library: PEC-3D.CEL
Cell Name: PIKEPL

1 REVISED 11-09-2011



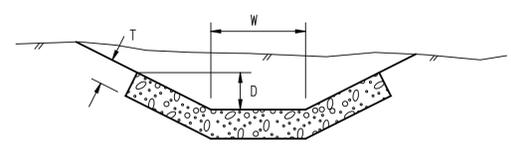
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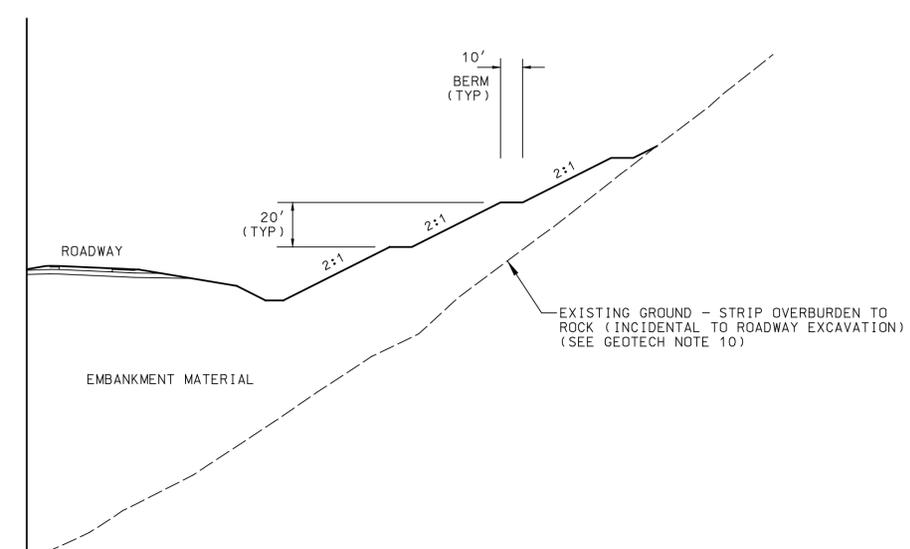
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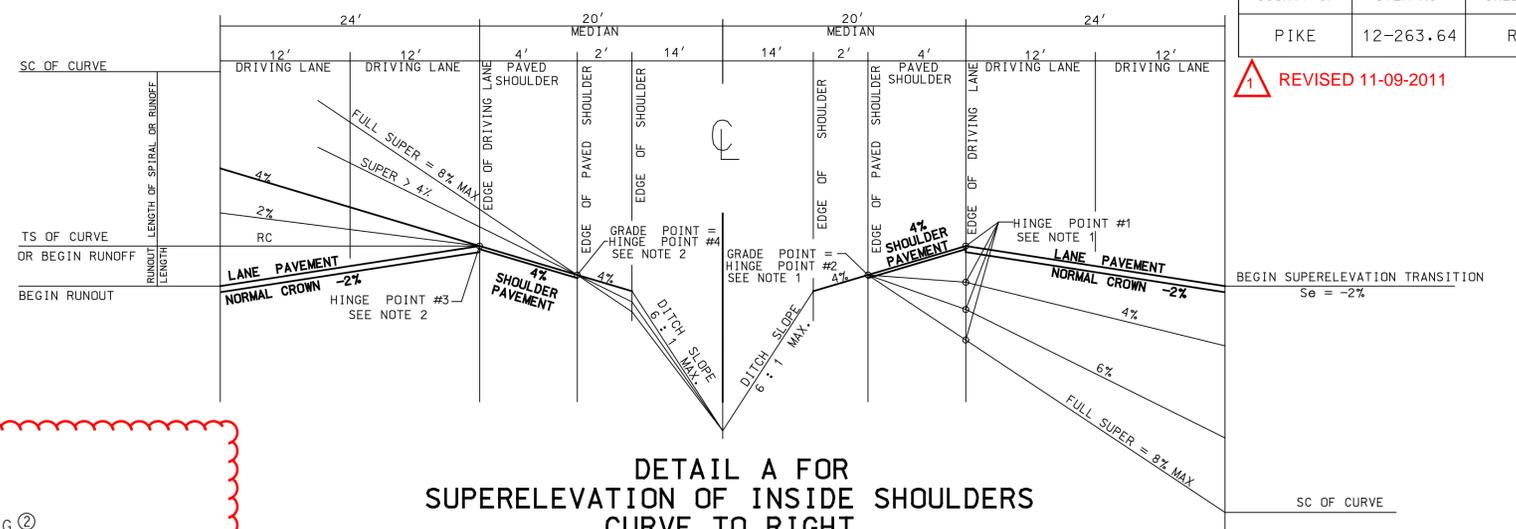


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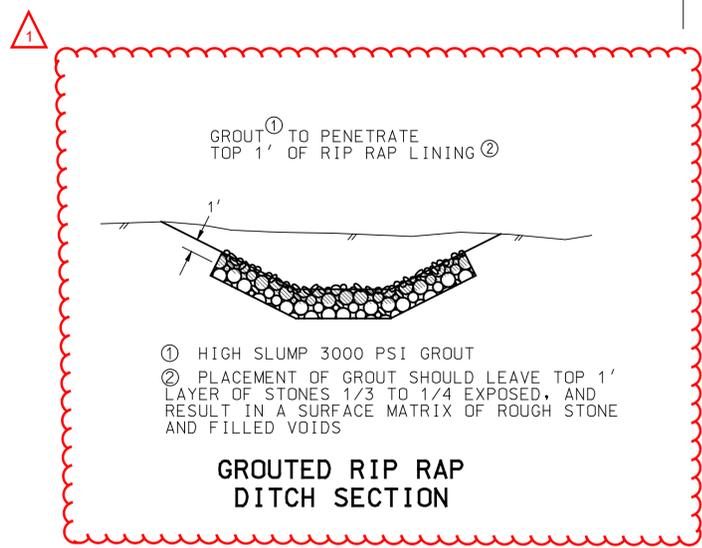
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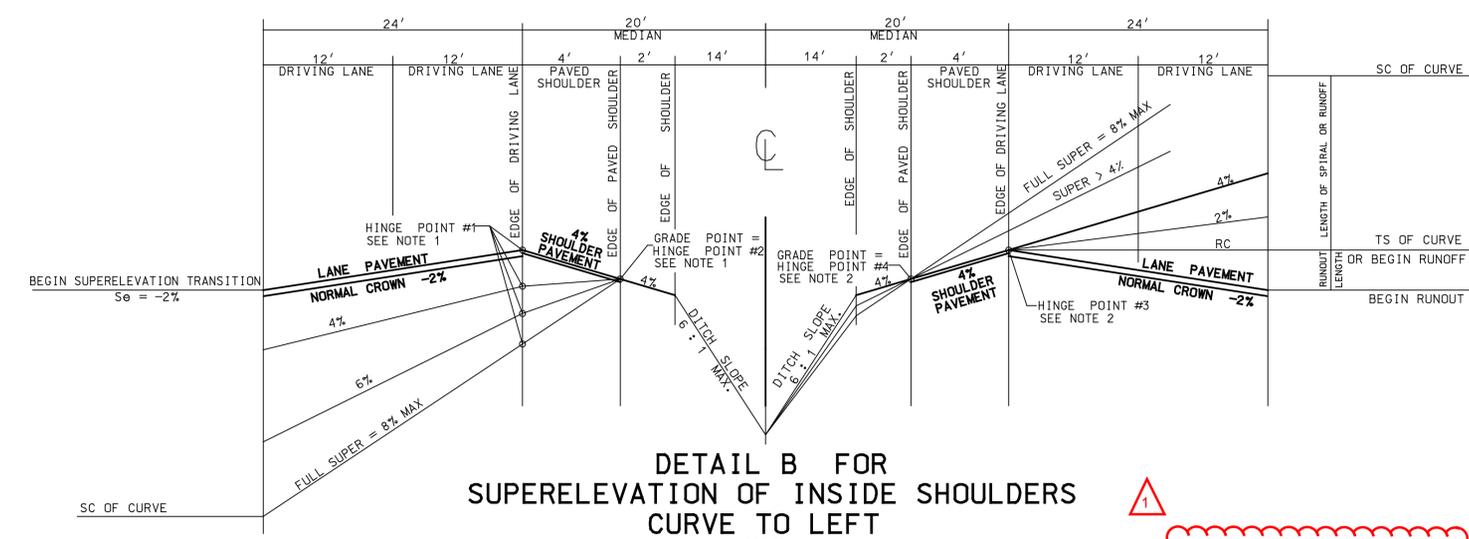
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1 GROUT TO PENETRATE TOP 1' OF RIP RAP LINING 2

- 1 HIGH SLUMP 3000 PSI GROUT
- 2 PLACEMENT OF GROUT SHOULD LEAVE TOP 1' LAYER OF STONES 1/3 TO 1/4 EXPOSED, AND RESULT IN A SURFACE MATRIX OF ROUGH STONE AND FILLED VOIDS

GROUTED RIP RAP DITCH SECTION



DETAIL B FOR SUPERELEVATION OF INSIDE SHOULDERS CURVE TO LEFT NTS

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1
ADDED GROUTED RIP RAP DITCH SECTION

PREPARED BY _____ DATE _____
CHECKED BY _____ DATE _____
APPROVED BY _____ DATE _____

Cell Library: PEC-3D.CEL
Cell Name: PIKEPL

COUNTY OF	ITEM NO.	SHEET NO.
PIKE	12-263.64	R44

GRADE AND DRAIN CONSTRUCTION NEAR CSXT RAILROAD

- 1) CONTRACTORS WILL PROVIDE CSXT WITH DETAILED PLANS FOR CONSTRUCTION OF RAILROAD CROSS PIPE, PIPE INLET AND OUTLET STRUCTURES, DITCHES, ROADWAY EXCAVATION, AND ROADWAY EMBANKMENT FOR APPROVAL PRIOR TO COMMENCEMENT OF WORK. PLANS WILL INCLUDE EXCAVATION AND EMBANKMENT PROCEDURES (INCLUDING BLASTING DETAILS), WORKING LIMITS, PROPOSED ACCESS POINTS, CONSTRUCTION SEQUENCE, CONSTRUCTION AND USAGE OF PROPOSED CROSSING, AND RAILROAD CROSS PIPE CONSTRUCTION DETAIL. ALL PLANS SUBJECT TO APPROVAL BY CSXT OR THEIR DESIGNATED REPRESENTATIVE AND THE ENGINEER.
- 2) THE CONTRACTORS SHALL TAKE ALL STEPS NECESSARY TO AVOID DAMAGE TO THE RAILS WHEN MOBILIZING VEHICLES ACROSS THE TRACKS. METHODS AND MATERIAL FOR CROSSINGS TO BE APPROVED IN ADVANCE BY CSXT AND THE ENGINEER.
- 3) PIPE CONSTRUCTION TO BE DONE IN ACCORDANCE TO CSXT'S DESIGN & CONSTRUCTION STANDARD SPECIFICATIONS FOR PIPELINE OCCUPANCIES, LAST REVISED 2/24/10. CONTRACTOR IS REQUIRED TO PROVIDE A TRACK MONITORING PROGRAM FOR PIPE CONSTRUCTION ACTIVITIES. PROGRAM SHALL BE IN ACCORDANCE TO CSXT'S CONSTRUCTION SUBMISSION CRITERIA, SECTION V - TRACK MONITORING, AS DATED 4/3/09. THE TUNNEL/BORING OPERATION SHALL BE PROCEEDED ON A 24-HOUR BASIS WITHOUT STOPPAGE WHILE WORKING WITHIN THE RAILROAD THEORETICAL LIVE LOAD INFLUENCE ZONE. AT ANY INTERRUPTION OF THE TUNNELING/BORING OPERATION, THE HEADING SHALL BE COMPLETELY BULKHEADED.
- 4) REFER TO THE CONSTRUCTION SUBMISSION CRITERIA SECTION IV - EXCAVATION AND SHORING, ITEM G, FOR BLASTING REQUIREMENTS OVER AND ABOVE CSX TRACKAGE.
- 5) CONTRACTOR MUST MAKE EFFORTS TO PROTECT CSX TRACKS BEFORE EACH BLASTING OCCURRENCE. TIME AND ACCESS FOR THE PLACEMENT AND REMOVAL OF THE PROTECTION SHALL BE DONE WITHIN THE TIME AS PERMITTED BY CSX FLAGMAN PRIOR TO EACH BLAST ACTIVITY. AT NO TIME WILL THE PROTECTION PREVENT TRAINS FROM MOVING THROUGH THE PROJECT AREA. PROTECTION MUST COMPLY WITH THE MINIMUM CLEARANCE REQUIREMENTS AS LISTED IN THE CSXT CONSTRUCTION SUBMISSION CRITERIA, LAST DATED 4/3/09.
- 6) EXCAVATION REQUIRED FOR THE ROADWAY, BRIDGE ABUTMENTS, FUTURE PIERS, AND DRAINAGE (INCLUDING BLASTING NEAR CSXT RIGHT OF WAY), IF IMPACTING CSXT PROPERTY, TO BE DONE IN ACCORDANCE WITH CSXT STANDARDS. EXCAVATION PROCEDURES TO BE APPROVED BY CSXT AND THE ENGINEER.
- 7) WHERE CRANES MAY BE USED TO MOBILIZE EQUIPMENT OR MATERIAL ON OR ACROSS CSXT PROPERTY, PLANS SHOULD INDICATE THE CAPACITY OF CRANES, LOCATION OF CRANES WITH RESPECT TO THE TRACKS, AND ESTIMATED LIFTING LOADS. THE ERECTION PROCEDURE TO BE APPROVED BY CSXT AND THE ENGINEER.
- 8) A CSXT FLAGMAN SHALL BE REQUIRED FOR ALL WORK ON OR NEAR CSXT RIGHT OF WAY, AND AT ALL TIMES LEADING UP TO AND FOLLOWING BLASTING IN THE AREA. REQUESTS FOR FLAGMAN SHOULD BE MADE 30 DAYS IN ADVANCE TO THE CSXT ROADMASTER, SHELBIANA, KY, OR THROUGH CSX'S DESIGNATED ENGINEERING REPRESENTATIVE. REQUESTS WILL BE HANDLED THROUGH THE PRESCRIBED FLAGMAN REQUEST FORM (TO BE PROVIDED AT THE PRECONSTRUCTION MEETING).
- 9) CONTRACTORS SHALL SUBMIT AS-BUILT DRAWINGS TO CSXT AND THE ENGINEER, INDICATING ACTUAL PIPE LOCATIONS AS CONSTRUCTED.
- 10) CONTRACTORS' PLAN SUBMISSIONS SHALL BE MADE A MINIMUM OF 30 DAYS PRIOR TO ANTICIPATED WORK. PLANS TO BE SUBMITTED TO CSXT CONSTRUCTION ENGINEERING, 1717 DIXIE HIGHWAY, SUITE 400, FT. WRIGHT, KY 41011 OR CSX'S DESIGNATED ENGINEERING REPRESENTATIVE.

TEMPORARY CROSSING
P.O.B. 0+00.00 = 385.00' LT STA. 733+00.00
P.C. 3+18.04
P.I. 3+86.25 = 542.82' LT STA. 736+60.00
P.T. 4+49.51
P.O.T. 5+76.74 = 497.03' LT STA. 738+50.00
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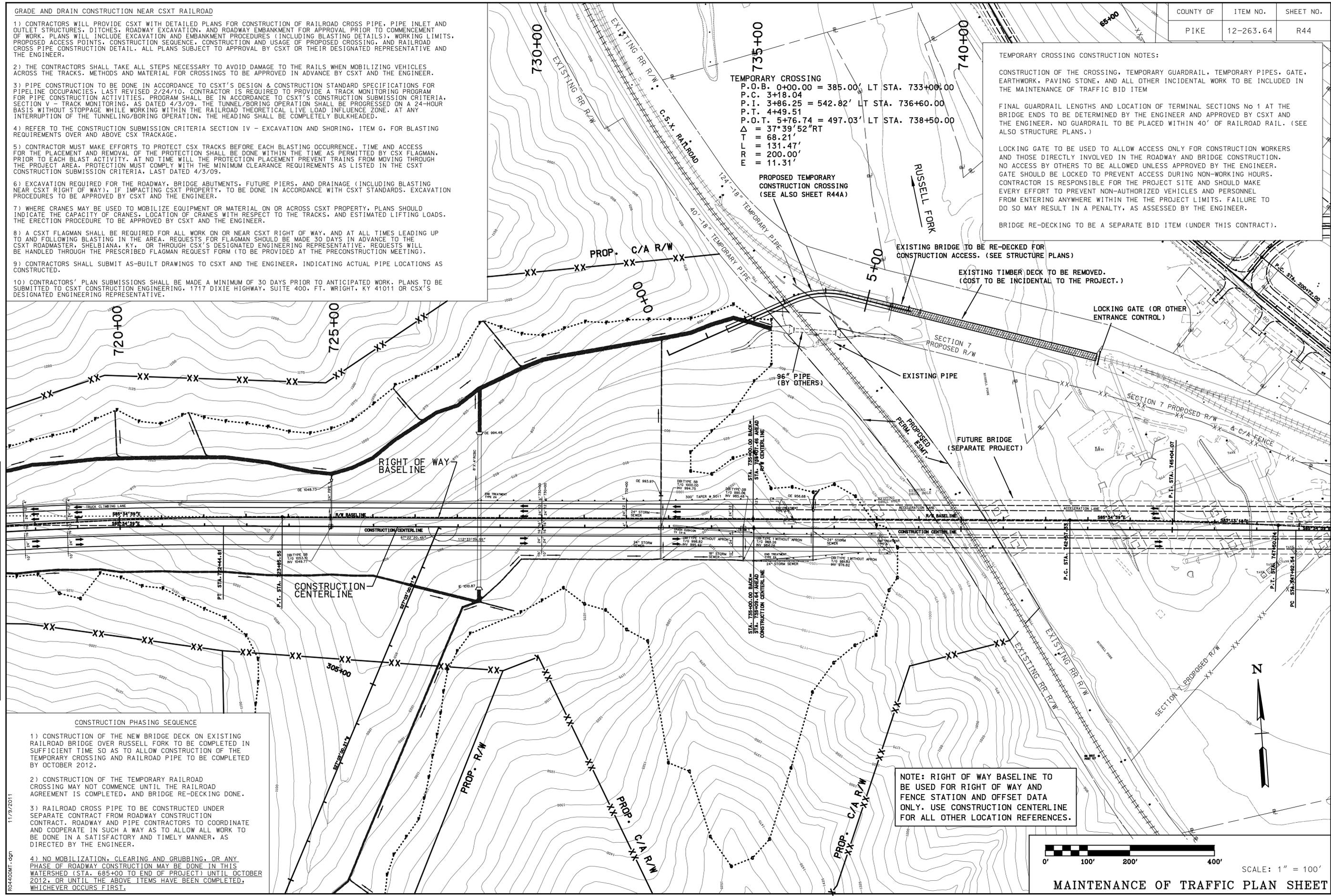
TEMPORARY CROSSING CONSTRUCTION NOTES:

CONSTRUCTION OF THE CROSSING, TEMPORARY GUARDRAIL, TEMPORARY PIPES, GATE, EARTHWORK, PAVING STONE, AND ALL OTHER INCIDENTAL WORK TO BE INCLUDED IN THE MAINTENANCE OF TRAFFIC BID ITEM

FINAL GUARDRAIL LENGTHS AND LOCATION OF TERMINAL SECTIONS No 1 AT THE BRIDGE ENDS TO BE DETERMINED BY THE ENGINEER AND APPROVED BY CSXT AND THE ENGINEER. NO GUARDRAIL TO BE PLACED WITHIN 40' OF RAILROAD RAIL. (SEE ALSO STRUCTURE PLANS.)

LOCKING GATE TO BE USED TO ALLOW ACCESS ONLY FOR CONSTRUCTION WORKERS AND THOSE DIRECTLY INVOLVED IN THE ROADWAY AND BRIDGE CONSTRUCTION. NO ACCESS BY OTHERS TO BE ALLOWED UNLESS APPROVED BY THE ENGINEER. GATE SHOULD BE LOCKED TO PREVENT ACCESS DURING NON-WORKING HOURS. CONTRACTOR IS RESPONSIBLE FOR THE PROJECT SITE AND SHOULD MAKE EVERY EFFORT TO PREVENT NON-AUTHORIZED VEHICLES AND PERSONNEL FROM ENTERING ANYWHERE WITHIN THE PROJECT LIMITS. FAILURE TO DO SO MAY RESULT IN A PENALTY, AS ASSESSED BY THE ENGINEER.

BRIDGE RE-DECKING TO BE A SEPARATE BID ITEM (UNDER THIS CONTRACT).



DATE _____
DATE _____
DATE _____

PREPARED BY _____
CHECKED BY _____
APPROVED BY _____

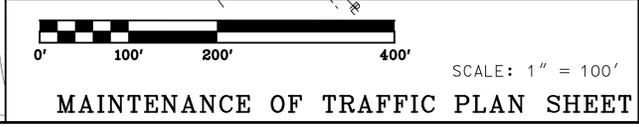
Cell Library: PEC-3D.CEL
Cell Name: PIKEPL

11/9/2011

6-93
FORM NO. 2m

- CONSTRUCTION PHASING SEQUENCE**
- 1) CONSTRUCTION OF THE NEW BRIDGE DECK ON EXISTING RAILROAD BRIDGE OVER RUSSELL FORK TO BE COMPLETED IN SUFFICIENT TIME SO AS TO ALLOW CONSTRUCTION OF THE TEMPORARY CROSSING AND RAILROAD PIPE TO BE COMPLETED BY OCTOBER 2012.
 - 2) CONSTRUCTION OF THE TEMPORARY RAILROAD CROSSING MAY NOT COMMENCE UNTIL THE RAILROAD AGREEMENT IS COMPLETED, AND BRIDGE RE-DECKING DONE.
 - 3) RAILROAD CROSS PIPE TO BE CONSTRUCTED UNDER SEPARATE CONTRACT FROM ROADWAY CONSTRUCTION CONTRACT. ROADWAY AND PIPE CONTRACTORS TO COORDINATE AND COOPERATE IN SUCH A WAY AS TO ALLOW ALL WORK TO BE DONE IN A SATISFACTORY AND TIMELY MANNER, AS DIRECTED BY THE ENGINEER.
 - 4) NO MOBILIZATION, CLEARING AND GRUBBING, OR ANY PHASE OF ROADWAY CONSTRUCTION MAY BE DONE IN THIS WATERSHED (STA. 685+00 TO END OF PROJECT) UNTIL OCTOBER 2012, OR UNTIL THE ABOVE ITEMS HAVE BEEN COMPLETED, WHICHEVER OCCURS FIRST.

NOTE: RIGHT OF WAY BASELINE TO BE USED FOR RIGHT OF WAY AND FENCE STATION AND OFFSET DATA ONLY. USE CONSTRUCTION CENTERLINE FOR ALL OTHER LOCATION REFERENCES.



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TEMPORARY CROSSING CONSTRUCTION NOTES: 1 **REVISED 11-09-2011**

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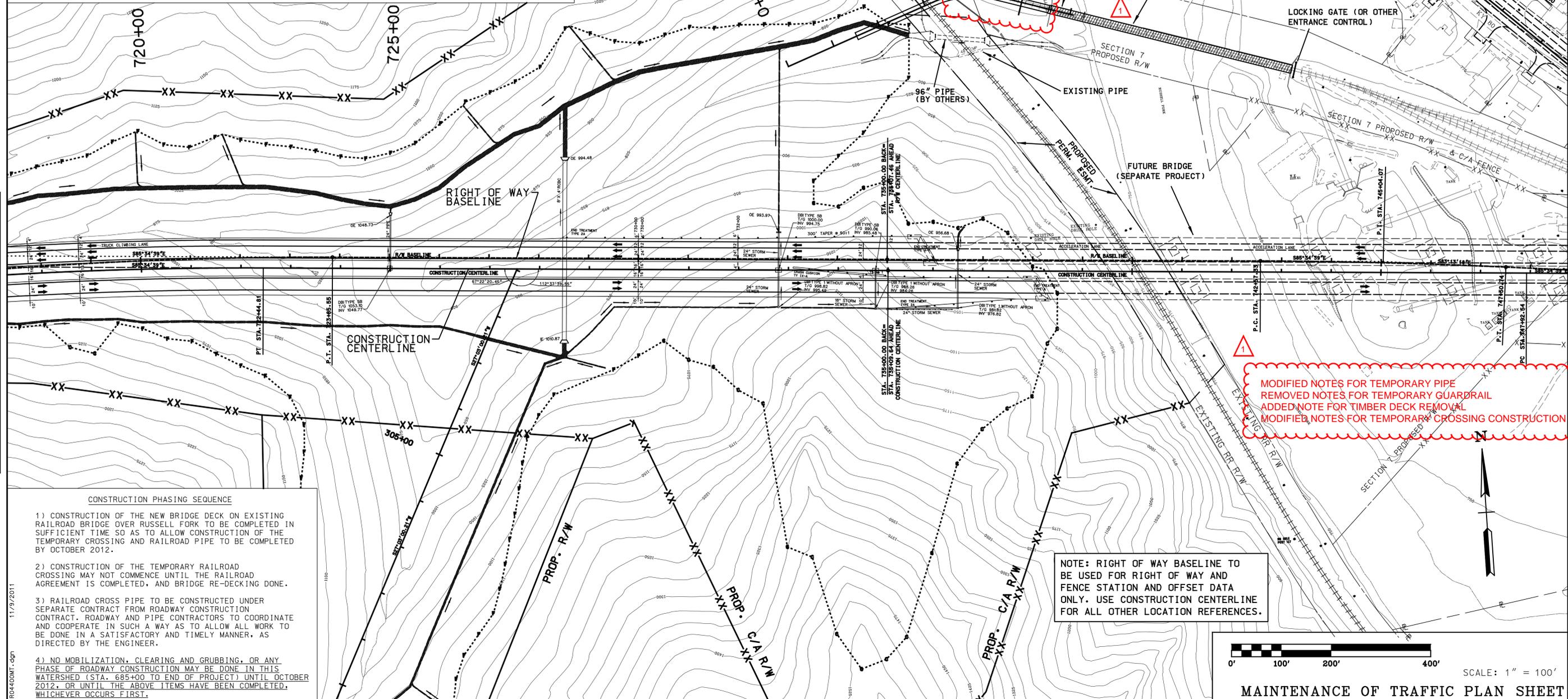
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BRIDGE RE-DECKING TO BE A SEPARATE BID ITEM (UNDER THIS CONTRACT).

1 EXISTING BRIDGE TO BE RE-DECKED FOR CONSTRUCTION ACCESS. (SEE STRUCTURE PLANS)

1 EXISTING TIMBER DECK TO BE REMOVED. (COST TO BE INCIDENTAL TO THE PROJECT.)



1 MODIFIED NOTES FOR TEMPORARY PIPE
REMOVED NOTES FOR TEMPORARY GUARDRAIL
ADDED NOTE FOR TIMBER DECK REMOVAL
MODIFIED NOTES FOR TEMPORARY CROSSING CONSTRUCTION

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DATE _____
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PREPARED BY _____
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APPROVED BY _____

Cell Library: PEC-3D.CEL
Cell Name: PIKEPL

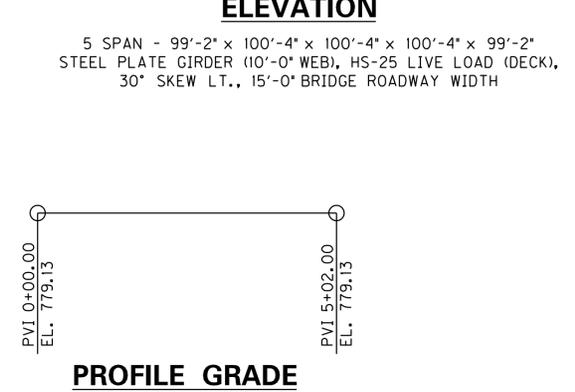
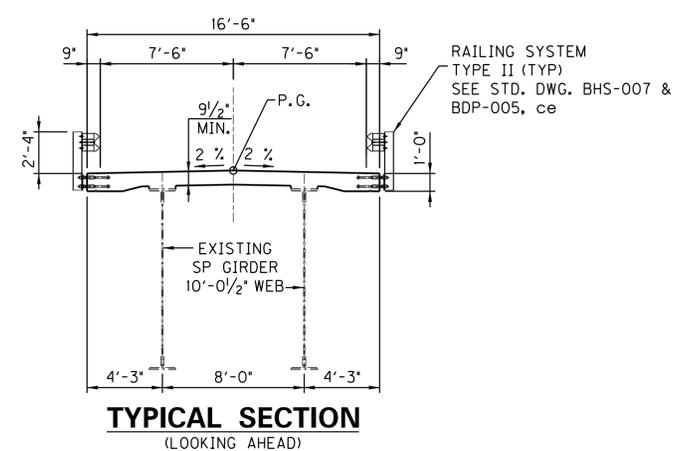
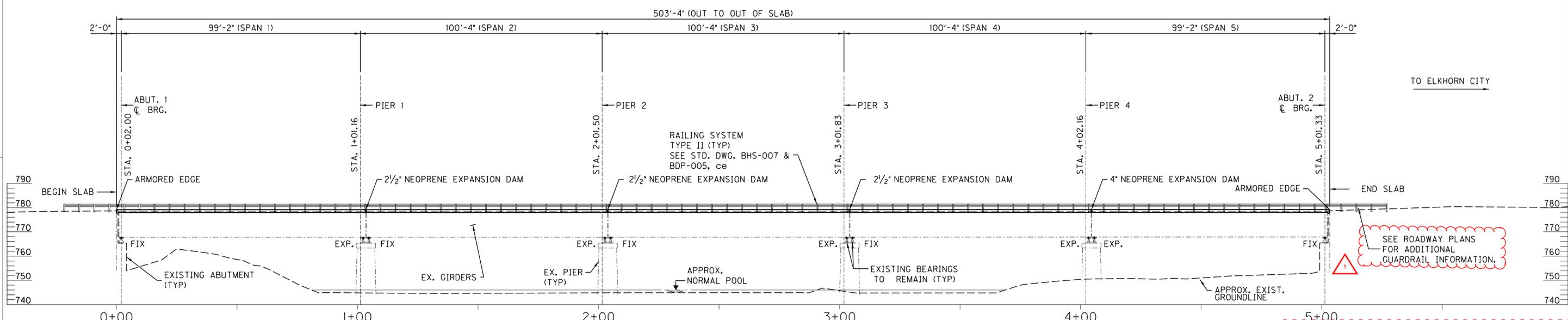
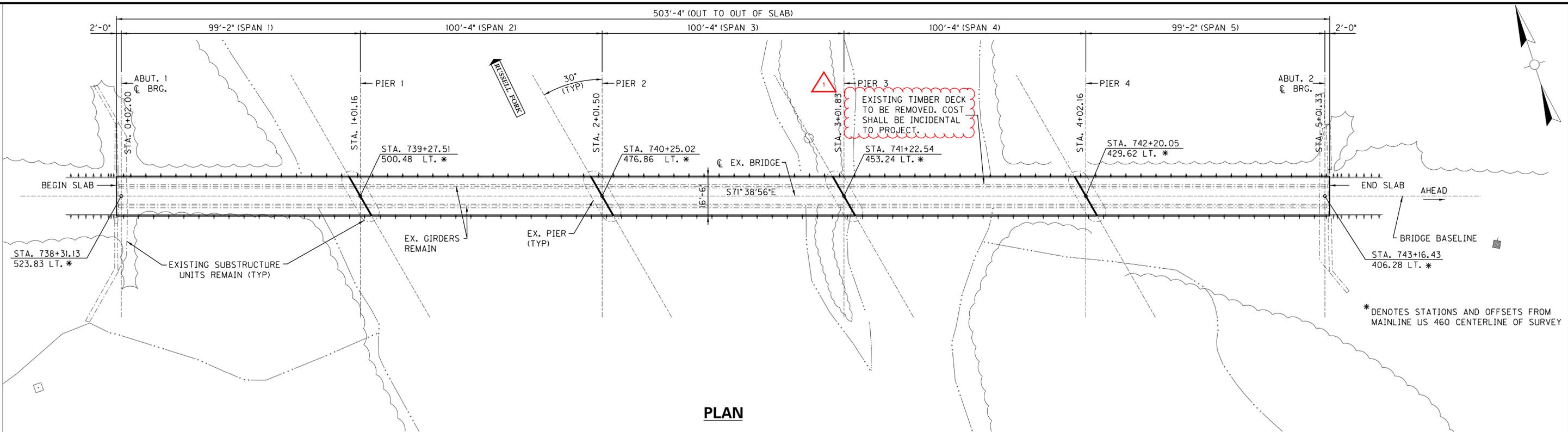
11/9/2011

6-93 FORM NO. 2m



SCALE: 1" = 100'

FILE NAME: J:\KDOT\US460-56\STRUCT\M.L.0.RUSSFRK.KY80\RAILROAD BRIDGE\AS SUBMITTED\PLAN REVISIONS 11-9-2011\26777-003.DGN
 USER: lucas-b
 DATE PLOTTED: November 9, 2011
 E-SHEET NAME: MicroStation v8.11.7.469



REVISOR: [Symbol] REVISED 11-09-2011

ITEM NUMBER	12-263.64
PREPARED BY	PALMER ENGINEERING CO.
SHEET NO.	S03
DRAWING NO.	26777

Revised Notes		11-9-11
REVISION	DATE	
DATE: JULY, 2011	CHECKED BY: L.A. CARLISLE	
DESIGNED BY: D.E. RUST	D.H. DEITZ	
DETAILED BY: J.A. ROSE		
Commonwealth of Kentucky		
DEPARTMENT OF HIGHWAYS		
COUNTY		
PIKE		
ROUTE CA	CROSSING	RUSSELL FORK
BRIDGE LAYOUT		

**SPECIAL NOTES FOR
PROTECTION OF RAILROAD INTEREST - CSXT**

1. AUTHORITY OF RAILROAD ENGINEER AND STATE ENGINEER:

The authorized representative of the Railroad Company, hereinafter referred to as Railroad Engineer, shall have final authority in all matters affecting the safe maintenance of Railroad traffic of his Company including the adequacy of the foundations and structures supporting the Railroad tracks.

The authorized representative of the State, hereinafter referred to as the Engineer, shall have authority over all other matters as prescribed herein and in the Project Specifications.

2. NOTICE OF STARTING WORK:

A. The Contractor shall not commence any work on Railroad rights of way until he has complied with the following conditions:

1. Given the Railroad written notice, with copy to the Engineer who has been designated to be in charge of the work, at least ten days in advance of the date he proposes to begin work on Railroad rights of way.

Les Scherr, Project Manager Public Projects
CSX Engineering Department
500 Water Street - J301
Jacksonville, Florida 32202
(P) 904-366-3057; (F) 904-366-4042

2. Notify the Railroad's Chief Regional Engineer's representative, **Jeremy Flynn, Roadmaster at Shelbiana, KY, (606) 437-4282**, at least 72 hours (not including Saturday, Sunday or Holidays) before proceeding with the work on Railroad property and shall abide by the instructions of said Railroad representative, insofar as the safety of the Railroad is concerned.
3. Obtain written authorization from the Railroad to begin work on Railroad rights of way, such authorization to include an outline of specific conditions with which he must comply.
4. Obtain written approval from the Railroad of Railroad Protective Insurance Liability coverage as required by paragraph 14 herein.
5. Furnish a schedule for all work within the Railroad rights of way as required by paragraph 7, B, 1.

- B. The Railroad's written authorization to proceed with the work shall include the names, addresses, and telephone numbers of the Railroad's representatives who are to be notified as hereinafter required. Where more than one representative is designated, the area of responsibility of each representative shall be specified.

3. INTERFERENCE WITH RAILROAD OPERATIONS:

- A. The Contractor shall so arrange and conduct his work that there will be no interference with Railroad operations, including train, signal, telephone and telegraphic services, or damage to the property of the Railroad Company or to poles, wires, and other facilities of tenants on the rights of way of the Railroad Company. Whenever work is liable to affect the operations or safety of trains, the method of doing such work shall first be submitted to the Railroad Engineer for approval, but such approval shall not relieve the Contractor from liability. Any work to be performed by the Contractor which requires flagging service or inspection service (watchman) shall be deferred by the Contractor until the flagging protection required by the Railroad is available at the job site.
- B. Whenever work within Railroad rights of way is of such a nature that impediment to Railroad operations such as use of runaround tracks or necessity for reduced speed is unavoidable, the Contractor shall schedule and conduct his operations so that such impediment is reduced to the absolute minimum.
- C. Should conditions arising from, or in connection with the work, require that immediate and unusual provisions be made to protect operations and property of the Railroad, the Contractor shall make such provisions. If in the judgment of the Railroad Engineer, or in his absence, the Engineer, such provisions are insufficient, either may require or provide such provisions, as he deems necessary. In any event, such unusual provisions shall be at the Contractor's expense and without cost to the Railroad or the State.

4. TRACK CLEARANCES

- A. The minimum track clearances to be maintained by the Contractor during construction are shown on the Project Plans. However, before undertaking any work within Railroad rights of way, or before placing any obstruction over any track, the Contractor shall:
 - 2. Notify the Railroad's representative at least 72 hours in advance of the work.
 - 3. Receive assurance from the Railroad's representative that arrangements have been made for flagging service as necessary.

4. Receive permission from the Railroad's representative to proceed with the work.
4. Ascertain that the Engineer has received copies of notice to the Railroad and of the Railroad's response thereto.

5. CONSTRUCTION PROCEDURES

A. General:

Construction work on Railroad property shall be:

1. Subject to the inspection and approval of the Railroad.
2. In accord with the Railroad's written outline of specific conditions.
3. In accord with the Railroad's general rules, regulations and requirements including those relating to safety, fall protection and personal protective equipment.
4. In accord with these Special Notes.

B. Excavation:

The subgrade of an operated track shall be maintained with edge of berm at least 10'0" from centerline of track and not more than 24 inches below top of rail. Contractor will not be required to make existing section meet this specification if substandard, in which case the existing section will be maintained.

C. Excavation of Structures:

The Contractor will be required to take special precaution and care in connection with excavating and shoring pits, and in driving piles, or sheeting for footings adjacent to tracks to provide adequate lateral support for the tracks and the loads which they carry, without disturbance of track alignment and surface, and to avoid obstructing track clearances with working equipment, tools or other material. The procedure for doing such work, including need of and plans for shoring, shall first be approved by the Engineer and the Railroad Engineer, but such approval shall not relieve the Contractor from liability.

D. Blasting:

1. The Contractor shall obtain advance approval of the Railroad Engineer and the Engineer for use of explosive on or adjacent to Railroad property. The request for permission to use explosives shall include a detailed blasting plan. If permission for use of explosives is granted, the Contractor will be required to comply with the following:
 - (a) Blasting shall be done with light charges under the direct supervision of a responsible officer or employee of the Contractor and a licensed blaster.
 - (b) Electric detonating fuses shall not be used because of the possibility of premature explosions resulting from operation of two-way train radios.
 - (c) No blasting shall be done without the presence of an authorized representative of the Railroad. At least 72 hours advance notice to the person designated in the Railroad's notice of authorization to proceed (see paragraph 2B above) will be required to arrange for the presence of an authorized Railroad representative and such flagging as the Railroad may require.
 - (d) Have at the job site adequate equipment, labor and materials and allow sufficient time to clean up debris resulting from the blasting without delay to trains, as well as correcting at his expense any track misalignment or other damage to Railroad property resulting from the blasting as directed by the Railway's authorized representative. If his actions result in delay of trains, the Contractor shall bear the entire cost thereof.
2. The Railroad representative will:
 - (a) Determine the approximate location of trains and advise the Contractor the approximate amount of time available for the blasting operation and clean-up.
 - (b) Have the authority to order discontinuance of blasting if, in his opinion, blasting is too hazardous or is not in accord with these Special Notes.

E. Maintenance of Railroad Facilities:

1. The Contractor will be required to maintain all ditches and drainage structures free of silt or other obstructions which may result from his operations and provide and maintain any erosion control measures as required. The Contractor will promptly repair eroded areas with Railroad rights of way and to repair any other damage to the property of the Railroad or its tenants.
2. All maintenance and repair of damages due to the Contractor's operations shall be done at the Contractor's expense.

F. Storage of Materials and Equipment:

Materials and equipment shall not be stored where they will interfere with Railroad operations, nor on the rights of way of the Railroad Company without first having obtained permission from the Railroad Engineer, and such permission will be with the understanding that the Railroad Company will not be liable for damage to such material and equipment from any cause and that the Railroad Engineer may move or require the Contractor to move, at the Contractor's expense, such material and equipment.

All grading or construction machinery that is left parked near the track unattended by a watchman shall be effectively immobilized so that it cannot be moved by unauthorized persons. The Contractor shall protect, defend, indemnify and save Railroad, and any associated, controlled or affiliated corporation, harmless from and against all losses, costs, expenses, claim or liability for loss or damage to property or the loss of life or personal injury, arising out of or incident to the Contractor's failure to immobilize grading or construction machinery.

G. Cleanup:

Upon completion of the work, the Contractor shall remove from within the limits of the Railroad rights of way, all machinery, equipment, surplus materials, falsework, rubbish or temporary buildings of the Contractor, and leave said rights of way in a neat condition satisfactory to the Chief Engineer of the Railroad or his authorized representative.

6. DAMAGES:

- A. The Contractor shall assume all liability for any and all damages to his work, employees, equipment and materials caused by Railroad traffic.

- B. Any cost incurred by the Railroad for repairing damages to its property or to property of its tenants, caused by or resulting from the operations of the Contractor, shall be paid directly to the Railroad by the Contractor.

7. FLAGGING SERVICES:

A. When Required:

Under the terms of the agreement between the Department and the Railroad, the Railroad has sole authority to determine the need for flagging required to protect its operations. In general, the requirements of such services will be whenever the Contractor's personnel or equipment are likely to be, working on the Railroad's rights of way, or across, over, adjacent to, or under a track, or when such work has disturbed or is likely to disturb a railroad structure or the railroad roadbed or surface and alignment of any track to such extent that the movement of trains must be controlled by flagging. **In the case of this project, flagging will likely be needed during blasting and excavation activities between Sta. 730+00 and the railroad and for construction of the temporary railroad crossing and installation of the new 96" pipe under the railroad.**

Normally, the Railroad will assign one flagman to a project; but in some cases, more than one may be necessary, such as yard limits where three- (3) flagmen may be required. However, if the Contractor works within distances that violate instructions given by the Railroad's authorized representative or performs work that has not been scheduled with the Railroad's authorized representative, a flagman or flagmen may be required until the project has been completed.

B. Scheduling and Notification:

1. Not later than the time that approval is initially requested to begin work on Railroad rights of way, Contractor shall furnish to the Railroad and the Department a schedule for all work required to complete the portion of the project within Railroad rights of way and arrange for a job site meeting between the Contractor, the Department, and the Railroad's authorized representative. Flagman or Flagmen may not be provided until the job site meeting has been conducted and the Contractor's work scheduled.
2. The Contractor will be required to give the Railroad representative at least 10 working days of advance written notice of intent to begin work within Railroad rights of way. Once begun, when work is suspended at any time for any reason, the Contractor will be required to give the Railroad

representative at least 3 working days of notice before resuming work on Railroad rights of way. Such notice shall include sufficient details of the proposed work to enable the Railroad representative to determine if flagging will be required. If such notice is in writing, the Contractor shall furnish the Engineer a copy; if notice is given verbally it shall be confirmed in writing with copy to the Engineer. If flagging is required, no work shall be undertaken until the flagman, or flagmen is present at the job site. It may take up to 30 days to obtain flagging initially from the Railroad. When flagging begins the flagman is usually assigned by the Railroad to work at the project site on a continual basis until no longer needed and cannot be called for on a spot basis. If flagging becomes unnecessary and is suspended, it may take up to 10 days to again obtain flagging services from the Railroad. Due to labor agreements, it is necessary to give 5 working days notice before flagging service may be discontinued and responsibility for payment stopped.

3. If, after the flagman is assigned to the project site, emergencies arise which require the flagman's presence elsewhere, and then the Contractor shall delay work on Railroad rights of way until such time as the flagman is again available. Any additional costs resulting from such delay shall be borne by the Contractor and not the Department or Railroad.

C. Payment:

1. The Department will pay the Railroad directly for the costs of flagging covered under the construction agreement between the Department and CSXT, which may be required to accomplish the construction. **This includes excavation between Sta. 730+00 and the railroad and for construction of the temporary at-grade construction crossing and installation of the new 96" pipe under the railroad.**
2. The estimated cost of flagging is \$585 per day based on Contractor's 8-hour work day, \$735 per 10-hour day, \$885 per 12-hour day, \$2,020 per 24-hour day (weekday), and \$3,110 per 24-hour day (weekend/holiday). This cost includes the base pay for the flagman, overhead, and includes a per diem charge for travel expenses, meals and lodging. The charge to the Department by the Railroad will be the actual cost based on the rate of pay for the Railroad's employees who are available for flagging service at the time the service is required.

3. Work by a flagman in excess of 8 hours per day or 40 hours per week, but not more than 12 hours a day will result in overtime pay at 1 ½ times the appropriate rate. Work by a flagman in excess of 12 hours per day will result in overtime pay at 2 times the appropriate rate. If work is performed on a holiday, the flagging rate is 2 ½ times the normal rate.
4. Railroad work involved in preparing and handling bills will also be charged to the Department. Charges to the Department by the Railroad shall be in accordance with applicable provisions of Subchapter B, Part 140, Subpart I and Subchapter G, Part 646, Subpart B of the Federal-Aid Policy Guide issued by the Federal Highway Administration on December 9, 1991, including all current amendments. Flagging costs are subject to change. The above estimates of flagging cost are provided for information only and are not binding in any way.

D. Verification:

1. The Department will review and sign the Railroad flagman's time sheet, attesting that the flagman was present during the time recorded. Flagman may be removed by Railroad if form is not signed. If flagman is removed, the Contractor will not be allowed to re-enter the Railroad rights of way until the issue is resolved. Any complaints concerning flagman or flagmen must be resolved in a timely manner. If need for flagman or flagmen is questioned, please contact Railroad's Projects Engineer (904) 359-1158. All verbal complaints must be confirmed in writing by the Contractor within 5 working days with copy to the Highway Engineer. All written correspondence should be addressed to:

Les Scherr, Project Manager Public Projects
CSX Engineering Department
500 Water Street - J301
Jacksonville, Florida 32202
(P) 904-366-3057; (F) 904-366-4042

2. The Railroad flagman assigned to the project will be responsible for notifying the Project Engineer upon arrival at the job site on the first day (or as soon thereafter as possible) that flagging services begin and on the last day that he performs such services for each separate period that services are provided. The Project Engineer will document such notification in the project records. When requested, the Project Engineer will also sign the flagman's diary showing daily time spent and activity at the project site.

8. HAUL ACROSS RAILROAD:

- A. Where the plans show or imply that materials of any nature must be hauled across a Railroad, unless the plans clearly show that the State has included arrangements for such haul in its agreement with the Railroad, the Contractor will be required to make all necessary arrangements with the Railroad regarding means of transporting such materials across the Railroad. The Contractor will be required to bear all costs incidental, including flagging, to such crossings whether services are performed by his own forces or by Railroad personnel.
- B. No crossing may be established for use of the Contractor for transporting materials or equipment across the tracks of the Railroad Company unless specific authority for its installation, maintenance, necessary watching and flagging thereof and removal, all at the expense of the Contractor, is first obtained from the Railroad Engineer.

9. WORK FOR THE BENEFIT OF THE CONTRACTOR:

- A. All temporary or permanent changes in wire lines or other facilities which are considered necessary to the project are shown on the plans; included in the force account agreement between the State and the Railroad or will be covered by appropriate revisions to same which will be initiated and approved by the State and/or the Railroad.
- B. Should the Contractor desire any changes in addition to the above, then he shall make separate arrangements with the Railroad for same to be accomplished at the Contractor's expense.

10. COOPERATION AND DELAYS:

- A. It shall be the Contractor's responsibility to arrange a schedule with the Railroad for accomplishing stage construction involving work by the Railroad or tenants of the Railroad. In arranging his schedule he shall ascertain, from the Railroad, the lead time required for assembling crews and materials and shall make due allowance therefore.
- B. No charge or claims of the Contractor against either the Department or the Railroad will be allowed for hindrance or delay on account of railroad traffic; any work done by the Railroad or other delay incident to or necessary for safe maintenance of Railroad traffic or for any delays due to compliance with these Special Notes.

11. TRAINMAN'S WALKWAYS:

Along the outer side of each exterior track of multiple operated track, and on each side of single operated track, an unobstructed continuous space suitable for trainman's use in walking along trains, extending to a line not less than 10 feet from centerline of track, shall be maintained. Any temporary impediments to walkways and track drainage encroachments or obstructions allowed during work hours while Railroad's protective service is provided shall be removed before the close of each day. If there is any excavation near the walkway, a handrail, with 10'-0" minimum clearance from centerline of track, shall be placed.

12. GUIDELINES FOR PERSONNEL ON RAILROAD RIGHTS OF WAY:

- A. All persons shall wear hard hats. Appropriate eye and hearing protection must be used. Working in shorts is prohibited. Shirts must cover shoulders, back and abdomen. Working in tennis or jogging shoes, sandals, boots with high heels, cowboy and other slip on type boots is prohibited. Hard-sole, lace-up footwear, zippered boots cinched with straps which fit snugly about the ankle are adequate. Safety boots are strongly recommended.
- B. No one is allowed within 25' of the centerline of the track without specific authorization from the flagman.
- C. All persons working near track when train is passing are to look out for dragging bands, chains and protruding or shifting cargo.
- D. No one is allowed to cross tracks without specific authorization from the flagman.
- E. All welders and cutting torches working within 25' of track must stop when train is passing.
- F. No steel tape or chain will be allowed to cross or touch rails without permission.

13. GUIDELINES FOR EQUIPMENT ON RAILROAD RIGHTS OF WAY:

- A. No crane or boom equipment will be allowed to set up to work or park within boom distance plus 15' of centerline of track without specific permission from railroad official and flagman.
- B. No crane or boom equipment will be allowed to foul track or lift a load over the track without flag protection and track time.
- C. All employees will stay with their machines when crane or boom equipment is pointed toward track.

- D. All cranes and boom equipment under load will stop work while a train is passing (including pile driving).
- E. Swinging loads must be secured to prevent movement while train is passing.
- F. No loads will be suspended above a moving train.
- G. No equipment will be allowed within 25' of centerline of track without specific authorization of the flagman.
- H. Trucks, tractors or any equipment will not touch ballast line without specific permission from railroad official and flagman.
- I. No equipment or load movement within 25' or above a standing train or other equipment without specific authorization of the flagman.
- J. All operating equipment within 25' of track must halt operations when a train is passing. All other operating equipment may be halted by the flagman if the flagman views the operation to be dangerous to the passing train.
- K. All equipment, loads and cables are prohibited from touching rails.
- L. While clearing and grubbing, no vegetation will be removed from railroad embankment with heavy equipment without specific permission from the Railroad Engineer and flagman.
- M. No equipment or materials will be parked or stored on Railroad's property unless specific permission is granted from the Railroad Engineer.
- N. All unattended equipment that is left parked on Railroad property shall be effectively immobilized so that it cannot be moved by unauthorized persons.
- O. All cranes and boom equipment will be turned away from track after each work day or whenever unattended by an operator.

14. INSURANCE:

- A. In addition to any other forms of insurance or bonds required under the terms of the contract and specifications, the Contractor will be required to carry insurance of the following kinds:
 - 1. Commercial General Liability coverage at their sole cost and expense with limits of not less than \$5,000,000 in combined single limits for bodily injury and/or property damage per occurrence, and such policies shall name the Railroad as an additional insured.

2. Statutory Worker's Compensation and Employers Liability Insurance with limits of not less than \$1,000,000, which insurance must contain a waiver of subrogation against the Railroad and its affiliates.
3. Commercial automobile liability insurance with limits of not less than \$1,000,000 combined single limit for bodily injury and/or property damage per occurrence, and such policies shall name the Railroad as an additional insured.
4. Railroad protective liability insurance with limits of not less than \$5,000,000 combined single limit for bodily injury and/or property damage per occurrence and an aggregate annual limit of \$10,000,000, which insurance shall satisfy the following additional requirements:
 - a. The Railroad Protective Insurance Policy must be on the ISO/RIMA Form of Railroad Protective Insurance - Insurance Services Office (ISO) Form CG 00 35.
 - b. The Railroad must be the named insured on the Railroad Protective Insurance Policy
 - c. Name and Address of the Contractor must be shown on the Declarations page.
 - d. Description of operations must appear on the Declarations page and must match the Project description, including project or contract identification numbers.
 - e. Authorized endorsements must include the Pollution Exclusion Amendment - CG 28 31, unless using form CG 00 35 version 96 and later.
 - f. Authorized endorsements may include:
 - i. Broad form Nuclear Exclusion - IL 00 21
 - ii. 30-day Advance Notices of Non-renewal or cancellation
 - iii. Required State Cancellation Endorsement
 - iv. Quick Reference or Index - CL/IL 240
 - g. Authorized endorsements may not include:
 - i. A Pollution Exclusion Endorsement except CG 28 31
 - ii. A Punitive or Exemplary Damages Exclusion
 - iii. A "Common Policy Conditions" Endorsement
 - iv. And endorsement that is not named in Section 4 (e) or (f) above.
 - v. Policies that contain any type of deductible

5. All insurance companies must be A. M. Best rated A- and Class VII or better.
 6. Such additional or different insurance as the Railroad may require.
- B. Additional Terms:
1. Contractor must submit the original Railroad Protective Liability policy, Certificates of Insurance, and all notices and correspondence regarding the insurance policy to the contact listed below.
 2. The Contractor may not begin work on the Project until it has received the Railroad's written approval or the required insurance.
- C. Insurance policies shall follow the requirements of Subchapter G, Part 646, Subpart A of the Federal-Aid Policy Guide issued by the Federal Highway Administration on December 9, 1991, including all current amendments.
- D. Evidence of insurance as required above shall be furnished to the address shown below for review by the Department and transmittal to the Railroad.
- E. If any part of the work is sublet, similar insurance and evidence thereof in the same amounts as required of the Prime Contractor shall be provided by or in behalf of the subcontractor to cover his operations. Endorsements to the Prime Contractor's policies specifically naming subcontractors and describing their operations will be acceptable for this purpose.
- F. All insurance herein before specified shall be carried until all work required to be performed under the terms of the contract has been satisfactorily completed within the limits of the rights of way of the Railroad as evidenced by the formal acceptance by the Department. Insuring Companies may cancel insurance by permission of the Department and Railroad or on thirty (30) days written notice to the Department and Railroad Insurance Contacts below.

Department:

Mr. Ryan Griffith, Director
Div. of Construction Procurement
KY Transportation Cabinet
200 Mero Street, 3rd Floor West
Frankfort, Kentucky 40622
Phone (502) 564-3500
Fax (502) 564-8961

Railroad:

Mr. Jonathan MacArthur
Manager-Insurance
CSX Corporation
500 Water Street - C907
Jacksonville, Florida 32202
Phone (904) 359-3394
Fax (904) 306-5325

15. FAILURE TO COMPLY:

These Special Notes are supplemental and amendatory to the Kentucky Department of Highways' Standard Specifications for Road and Bridge Construction, Edition of 2004, and amendments thereof, and where in conflict therewith, these Special Notes shall govern.

In the event the Contractor violates or fails to comply with any of the requirements of these Special Notes:

- A. The Railroad Engineer may require that the Contractor vacate Railroad property.
- B. The Engineer may withhold all monies due the Contractor on monthly statements.

Any such orders shall remain in effect until the Contractor has remedied the situation to the satisfaction of the Railroad Engineer and the Engineer.

16. PAYMENT FOR COST OF COMPLIANCE:

No separate payment will be made for any extra cost incurred on account of compliance with these Special Notes. All such cost shall be included in prices bid for other items of the work as specified in the payment items.

Office of the Principal Engineer Public Projects
CSX Engineering Department
500 Water Street - J301
Jacksonville, Florida 32202

Date: November 10, 2011
File: Elkhorn City, Kentucky
Milepost: CMG-126.8
DOT# New (not assigned)

FLAGMEN AND PROTECTIVE SERVICES SCHEDULE OF RATES AND RELATED COSTS

LOCATION: Elkhorn City, Pike County, Kentucky **DATE:** September 23, 2011

PROJECT: Proposed roadway construction, US 460, adjacent to CSXT @ MP CMG – 126.8,
OP#: KY0229

SERVICES: Flagmen required by Railroad to protect its operations and property will be
furnished by Railroad at the following rates and costs.

CRAFTS & RATES:	<u>Craft</u>	<u>No.</u>	<u>Hourly Rate</u>
	Foreman/Flagman	1	\$25.00 *
	* Estimated Daily Rate:(8 hours (contractor work day), plus 1 hr. travel to and from headquarters, & 2 hrs. to install and remove warning boards, additives, and travel expenses is approximately \$585 per 8 hr day, \$735 per 10 hr day, \$885 per 12 hr day, \$2,020 per 24 hr day (weekday) and \$3,110 per 24 hr day (weekend/holiday).		

BASE RATE: Reimbursement is required for full eight-hour day for any flagman furnished
unless said flagman is assigned to other work during a portion of such day, in
which event reimbursement will not be required for the portion of the day said
flagman is engaged in other work.

PUNITIVE RATE: One and one-half (1½) times the hourly rate in excess of eight (8) hours
and two times (2) in excess of sixteen (16) on Monday through Friday
and One and one-half (1 ½) for any time on Saturday, Sunday and
Holidays.

ADDITIVES: The composite percentage added to rates is **119.95%** and includes Railroad
Retirement and Unemployment Tax, Vacation, Holidays, Health and Welfare,
Workmen’s Compensation, Supervision, and Small Tools & Safety Training and
other miscellaneous items.

EXPENSES: Actual cost for travel, including meals, lodging and transportation will be charged.
Reimbursement for use of private automobile is allowed at **55** cents per mile or
Contractor may be required to provide transportation from **Shelbania, KY** to the
site and return.

NOTIFICATION: CSX contact employee: **Jeremy Flynn - Roadmaster at Shelbiana, KY,**
(606) 437-4282 must be notified at least **28 days** (not including Saturday,
Sunday and Holiday) in advance of the need of full time protection as
required.
Division: **Appalachian**
Subdivision: **Big Sandy**

SHELTER: Contractor shall provide Flagmen with a heated shelter and suitable sanitation
facilities when necessary.

TRAIN TRAFFIC:	6AM TO 6 PM	6PM TO 6 AM	Maximum Speed
Freight	15	12	30 MPH
Passenger			

Special Note for Temporary Railroad Crossing

Application for Crossing

The Contractor shall procure the temporary railroad crossing, as shown on Sheet R44 of the plans. The crossing has been negotiated with CSXT and has been assigned a CSX Agreement # CSX689761. The awarded Contractor is to complete the following form and submit it to Ms. Sherry Millard, CSX Transportation, Property Management J180, 6737 Southpoint Drive S, Suite 100, Jacksonville, Florida 32202; Office # (904) 279-3872; email: Sherry.Millard@csx.com. A copy should also be sent to Allen Rust, Rail Coordinator, Division of Right of Way and Utilities, 5th Floor, 200 Mero Street, Frankfort, Kentucky 40622.

The Contractor will be responsible for the \$2,500 License Fee but the \$1,250 Application Fee will be waived. The costs associated with constructing the temporary crossing are to be paid by the Contractor. The crossing costs \$700/ft and comes in 8 foot sections. The Contractor will also be responsible for a \$10,000 removal fee that is due up front.

Flagging and Safety Devices

CSXT may require flagging protection or temporary signals to be installed at the temporary crossing to ensure safety. This is to be coordinated with CSXT Property Management at the time of application for the crossing. When a flagger is already present on the site for either blasting and excavation activities between Sta. 730+00 and the railroad or for construction of the temporary railroad crossing and installation of the new 96" pipe under the railroad, he or she may also guard the RR crossing. These flagging costs would be paid by the Cabinet. However, if a flagger is not needed for these specific activities, the costs for a flagger needed solely for the crossing are to be paid by the Contractor. Flagging for a temporary crossing costs approximately \$1,000 per day.

The Contractor may elect to install automatic signals and/or gates at the temporary crossing to reduce flagging costs when not working in the defined areas. If the decision is made to install signals and/or gates, the Contractor is to coordinate their installation with CSXT during the application process. The signals/gates apparatus costs approximately \$350,000 to install and the cost would be the responsibility of the Contractor.



APPLICATION FOR PRIVATE ROAD GRADE CROSSINGS (Existing Crossing)

ROAD CROSSING POLICY

It is the policy of CSX Transportation, Inc., to enhance public safety and the safety of its operations. As such, CSX Transportation, Inc., will refuse to allow any *new* private road crossings of rail lines unless the party requesting the crossing is able to definitely demonstrate that such crossing is either required by law, deed, or charter, is needed by a CSXT customer, or is needed for access to "landlocked" property. For *existing crossings*, if a party demonstrates that a particular property is landlocked, CSX Transportation, Inc., may allow the private road crossing to remain if, and only if, the private property owner is willing to enter into a Private Road Crossing Agreement providing CSX Transportation with, among other things, indemnification, insurance, maintenance, and termination protections. Where reasonable alternative access to private property is available, however, a Private Road Crossing Agreement will not be offered or granted.

REQUIRED AGREEMENT FEES

Application Fee:

A one time non-refundable application fee of \$1,250.00, will be required to process your application.

Annual License Fee:

An annual license fee will be required for all Private Road Crossings.

Annualized Replacement Cost:

Annualized replacement costs, based upon 1/10th of the estimated cost of the crossing installation will be charged to the applicant. This charge is subject to yearly adjustment based on annual increase to Consumers Price Index. Fees are payable in advance, the first payment being made at the time the agreement is executed.

OTHER REQUIREMENTS

If a crossing agreement is granted, applicant must maintain approaches, drainage and adequate sight clearance. The Applicant will be required to carry liability insurance coverage during the life of this agreement. This insurance automatically will cover liability, which the Applicant assumes under the agreement. All agreements will contain a future crossing signalization clause, requiring automatic traffic control devices at the entire expense of applicant, including maintenance, if the need develops or if required by a governmental agency or by CSX Transportation, Inc.



TEMPORARY CONSTRUCTION
APPLICATION FOR ~~PRIVATE ROAD CROSSING AGREEMENT~~
~~(Existing Crossing Without Agreement)~~

1. Complete legal name of Applicant (exactly as it should appear on the Agreement)

Applicant: _____

Individual: _____ Corporation: _____ State of Incorporation: _____ Municipality or Government Agency: _____
Partnership: Type _____ State of Partnership: _____

Address: _____

City: _____ State _____ Zip Code _____

Telephone Number: (_____) - _____ - _____ Fax Number: (_____) - _____ - _____

2. Location of crossing is approx. 0.00 feet (CIRCLE) NORTH SOUTH EAST WEST from Milepost CMG - 126.8
Location Elkhorn City County Pike State Kentucky

3. State reason for crossing: (including frequency of use)**

A temporary construction crossing is needed to move personnel and light vehicles only. No materials will be moved across the crossing. **This crossing is associated with CSX Agreement # CSX689761.**

4. Is other access available? (Circle) Yes No If property recently purchased, why was access not obtained from Seller?
Access is available, but not always feasible. This crossing has been negotiated and associated with the above agreement.

5. Number of tracks crossed: 1 **Width of crossing:** _____

*** NOTE CSXT POLICY STATEMENT ON PAGE 2 OF FORM CSXT-7459A AND BE GOVERNED ACCORDINGLY ***

I understand that by making an application, if approved, I will be sent a proposed agreement for my review and execution. This agreement may include an Annual License Fee and an Annualized Replacement Cost of the crossing, Insurance, and will outline the division of responsibilities and liabilities between both parties.

Applicant's signature: _____ Date _____

Please Type or Print Name: _____ Title: _____

RAILROAD USE ONLY:

Train Speed _____ Milepost _____
Crossing Signals Required? Yes / No _____
DOT/AAR Inventory Number Assigned _____ Valuation Stationing _____
When applicable, OSP Number _____ Valuation Map _____
Division _____ Subdivision _____ Crossing Width _____
Forwarded By: _____ CSX ROADXG _____ Date: _____

ERRATUM

Refer to the Locality Number and Determination Number listed below published by the Kentucky Labor Cabinet, Division of Employment Standards, Apprenticeship and Mediation dated August 4, 2011.

Locality: Highway Construction Locality No. II, including the following counties: Adair, Barren, Breathitt, Casey, Clay, Clinton, Cumberland, Estill, Floyd, Garrard, Green, Harlan, Hart, Jackson, Johnson, Knott, Knox, Laurel, Lawrence, Lee, Leslie, Letcher, Lincoln, McCreary, Magoffin, Martin, Menifee, Metcalfe, Monroe, Morgan, Owsley, Perry, Pike, Powell, Pulaski, Rockcastle, Russell, Taylor, Wayne, Whitley and Wolfe.

Determination Number: CR-II-II-HWY

DELETE:

Ironworkers	BASE RATE	\$25.77
	FRINGE BENEFIT	18.54

INSERT:

Ironworker (Structural)	BASE RATE	\$22.50
	FRINGE BENEFIT	8.75

Ironworker (Reinforcing)	BASE RATE	\$22.30
	FRINGE BENEFIT	8.75



Michael L. Dixon, Commissioner
Department of Workplace Standards
Kentucky Labor Cabinet
Frankfort, KY 40601

This 8th day of November, 2011.

CONTRACT ID: 111044
 COUNTY: PIKE
 PROPOSAL: APD 0806 (039)

PAGE: 1
 LETTING: 11/18/11
 CALL NO: 101

LINE NO	ITEM	DESCRIPTION	APPROXIMATE QUANTITY	UNIT	UNIT PRICE	AMOUNT
SECTION 0001 ROADWAY						
0010	00020	TRAFFIC BOUND BASE	333.000	TON		
0020	00462	CULVERT PIPE-18 IN	77.000	LF		
0030	00469	CULVERT PIPE-42 IN	96.000	LF		
0040	00471	CULVERT PIPE-54 IN	730.000	LF		
0050	00522	STORM SEWER PIPE-18 IN	219.000	LF		
0060	00524	STORM SEWER PIPE-24 IN	1,639.000	LF		
0070	00526	STORM SEWER PIPE-30 IN	1,085.000	LF		
0080	00528	STORM SEWER PIPE-36 IN	1,491.000	LF		
0090	00529	STORM SEWER PIPE-42 IN	1,094.000	LF		
0100	00530	STORM SEWER PIPE-48 IN	903.000	LF		
0110	00531	STORM SEWER PIPE-54 IN	1,844.000	LF		
0120	00532	STORM SEWER PIPE-60 IN	185.000	LF		
0130	01000	PERFORATED PIPE-4 IN	440.000	LF		
0140	01002	PERFORATED PIPE-8 IN	440.000	LF		
0150	01010	NON-PERFORATED PIPE-4 IN	485.000	LF		
0160	01012	NON-PERFORATED PIPE-8 IN	580.000	LF		
0170	01024	PERF PIPE HEADWALL TY 2-4 IN	1.000	EACH		
0180	01032	PERF PIPE HEADWALL TY 4-4 IN	3.000	EACH		
0190	01490	DROP BOX INLET TYPE 1	18.000	EACH		
0200	01493	DROP BOX INLET TYPE 2	9.000	EACH		

CONTRACT ID: 111044
COUNTY: PIKE
PROPOSAL: APD 0806 (039)

PAGE: 2
LETTING: 11/18/11
CALL NO: 101

LINE NO	ITEM	DESCRIPTION	APPROXIMATE QUANTITY	UNIT	UNIT PRICE	AMOUNT
0210	01505	DROP BOX INLET TYPE 5B	9.000	EACH		
0220	02200	ROADWAY EXCAVATION	20,953,132	CUYD		
0230	02262	FENCE-WOVEN WIRE TYPE 1	28,000.000	LF		
0240	02351	GUARDRAIL-STEEL W BEAM-S FACE	12.500	LF		
0250	02360	GUARDRAIL TERMINAL SECTION NO 1	2.000	EACH		
0260	02429	RIGHT-OF-WAY MONUMENT TYPE 1	31.000	EACH		
0270	02432	WITNESS POST	3.000	EACH		
0280	02475	PLUG WATER WELL	4.000	EACH		
0290	02483	CHANNEL LINING CLASS II	14,511.000	TON		
0300	02488	CHANNEL LINING CLASS IV	28,175.000	CUYD		
0310	02542	CEMENT	10.000	TON		
0320	02545	CLEARING AND GRUBBING 403 ACRES	(1.00)	LS		
0330	02562	SIGNS	101.000	SQFT		
0340	02600	FABRIC GEOTEXTILE TY IV FOR PIPE	26,345.000	SQYD	2.00	52,690.00
0350	02610	RETAINING WALL-GABION	8,243.000	CUYD		
0360	02650	MAINTAIN & CONTROL TRAFFIC	(1.00)	LS		
0370	02701	TEMP SILT FENCE	18,200.000	LF		
0380	02703	SILT TRAP TYPE A	82.000	EACH		
0390	02704	SILT TRAP TYPE B	82.000	EACH		
0400	02705	SILT TRAP TYPE C	41.000	EACH		
0410	02706	CLEAN SILT TRAP TYPE A	492.000	EACH		

CONTRACT ID: 111044
 COUNTY: PIKE
 PROPOSAL: APD 0806 (039)

PAGE: 3
 LETTING: 11/18/11
 CALL NO: 101

LINE NO	ITEM	DESCRIPTION	APPROXIMATE QUANTITY	UNIT	UNIT PRICE	AMOUNT
0420	02707	CLEAN SILT TRAP TYPE B	492.000	EACH		
0430	02708	CLEAN SILT TRAP TYPE C	246.000	EACH		
0440	02709	CLEAN TEMP SILT FENCE	109,200.000	LF		
0450	02711	SEDIMENTATION BASIN	48,267.000	CUYD		
0460	02712	CLEAN SEDIMENTATION BASIN	96,534.000	CUYD		
0470	02726	STAKING	(1.00)	LS		
0480	05950	EROSION CONTROL BLANKET	1,000.000	SQYD		
0490	05952	TEMP MULCH	1,950,520.000	SQYD		
0500	05953	TEMP SEEDING AND PROTECTION	105,512.000	SQYD		
0510	05966	TOPDRESSING FERTILIZER	60.000	TON		
0520	05985	SEEDING AND PROTECTION	1,055,120.000	SQYD		
0530	08100	CONCRETE-CLASS A	48.630	CUYD		
0540	08100	CONCRETE-CLASS A FOR CAPPING OF VERTICAL SHAFTS	10.000	CUYD		
0550	08150	STEEL REINFORCEMENT	3,339.000	LB		
0560	08150	STEEL REINFORCEMENT FOR CAPPING OF VERTICAL SHAFTS	1,080.000	LB		
0570	10020NS	FUEL ADJUSTMENT	3,000,489.000	DOLL		
0580	20667ED	PNEUMATIC BACKSTOWING	1,000.000	TON		
0590	20911ED	HIGH SLUMP 3000 PSI GROUT	1,615.000	CUYD		
0600	22628NN	DROP BOX INLET-MOD	21.000	EACH		
0610	23131ER701	PIPELINE VIDEO INSPECTION	4,682.000	LF		

SECTION 0002 BRIDGE-CULVERT

CONTRACT ID: 111044
COUNTY: PIKE
PROPOSAL: APD 0806 (039)

PAGE: 4
LETTING: 11/18/11
CALL NO: 101

LINE NO	ITEM	DESCRIPTION	APPROXIMATE QUANTITY	UNIT	UNIT PRICE	AMOUNT
0620	02355	GUARDRAIL-STEEL W BEAM-S FACE A	100.000	LF		
0621	02360	GUARDRAIL TERMINAL SECTION NO 1 (ADDED: 11-10-11)	4.000	EACH		
0630	03299	ARMORED EDGE FOR CONCRETE	33.000	LF		
0640	08003	FOUNDATION PREPARATION 26775	(1.00)	LS		
0650	08003	FOUNDATION PREPARATION 26776	(1.00)	LS		
0660	08100	CONCRETE-CLASS A	622.000	CUYD		
0670	08104	CONCRETE-CLASS AA	280.000	CUYD		
0680	08150	STEEL REINFORCEMENT	78,126.000	LB		
0690	08151	STEEL REINFORCEMENT-EPOXY COATED	101,073.000	LB		
0700	08471	EXPANSION DAM-2.5 IN NEOPRENE	58.000	LF		
0710	08472	EXPANSION DAM-4 IN NEOPRENE	20.000	LF		
0720	08801	GUARDRAIL-STEEL W BEAM-S FACE BR	1,004.000	LF		
SECTION 0003 TRAINEES						
0730	02742	TRAINEE PAYMENT REIMBURSEMENT 2 ARTICULATING OFF ROAD TRUCK DRIVERS	3,200.000	HOUR		
0740	02742	TRAINEE PAYMENT REIMBURSEMENT 2 CLASS A OR B OPERATORS	3,200.000	HOUR		
SECTION 0004 MOB AND DEMOB						
0750	02568	MOBILIZATION (NO MORE THAN 5%)		LUMP		
0760	02569	DEMOBILIZATION (AT LEAST 1.5%)		LUMP		
TOTAL BID						